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# NOTICE OF MEETING

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## **CABINET MEMBER FOR TRAFFIC & TRANSPORTATION**

**THURSDAY, 13 SEPTEMBER 2018 AT 5.00 PM**

**THE EXECUTIVE MEETING ROOM - THIRD FLOOR, THE GUILDHALL**

Telephone enquiries to Joanne Wildsmith, Democratic Services Tel: 9283 4057

Email: [joanne.wildsmith@portsmouthcc.gov.uk](mailto:joanne.wildsmith@portsmouthcc.gov.uk)

If any member of the public wishing to attend the meeting has access requirements, please notify the contact named above.

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## **CABINET MEMBER FOR TRAFFIC & TRANSPORTATION**

Councillor Lynne Stagg (Liberal Democrat)

### **Group Spokespersons**

Councillor Simon Boshier, Conservative

Councillor Yahiya Chowdhury, Labour

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(NB This Agenda should be retained for future reference with the minutes of this meeting.)

Please note that the agenda, minutes and non-exempt reports are available to view online on the Portsmouth City Council website: [www.portsmouth.gov.uk](http://www.portsmouth.gov.uk)

**Deputations by members of the public may be made on any item where a decision is going to be taken. The request should be made in writing to the contact officer (above) by 12 noon of the working day before the meeting, and must include the purpose of the deputation (for example, for or against the recommendations). Email requests are accepted.**

## **A G E N D A**

- 1 Apologies**
- 2 Declarations of Members' Interests**
- 3 Fair Oak Road and Battenburg Avenue Double Yellow Line Proposals (TRO 53b/2018) (Pages 3 - 14)**

The purpose of the report by the Director of Regeneration is to consider public responses to proposed new double yellow lines in Fair Oak Road and a proposed reduction of double yellow lines in Battenburg Avenue.

### **RECOMMENDED:**

- (1) That the double yellow lines proposed under TRO 53/2018 in Fair Oak Road are implemented as advertised;**
- (2) That the reduction of double yellow lines proposed under TRO 53/2018 in Battenburg Avenue, near its junction with Randolph Road, is not implemented in full, but that;**
- (3) The 6-metre length of double yellow lines is removed from across the driveway between Nos.38 and 40 Battenburg Avenue.**

**4 Proposed Residents' Parking Zone FH: Twyford Avenue (TRO 76/2018)**  
(Pages 15 - 26)

The purpose of the report by the Director of Regeneration is to consider the public response to the proposed residents' parking zone at the northern end of Twyford Avenue.

**RECOMMENDED that the residents' parking zone proposed under Traffic Regulation Order 76/2018 is implemented as advertised (FH Permit Holders Only 2pm-10pm).**

Members of the public are permitted to use both audio visual recording devices and social media during this meeting, on the understanding that it neither disrupts the meeting nor records those stating explicitly that they do not wish to be recorded. Guidance on the use of devices at meetings open to the public is available on the Council's website and posters on the wall of the meeting's venue.

Whilst every effort will be made to webcast this meeting, should technical or other difficulties occur, the meeting will continue without being webcast via the Council's website.

This meeting is webcast (videoed), viewable via the Council's livestream account at <https://livestream.com/accounts/14063785>

**Title of meeting:** Cabinet Member for Traffic and Transportation Decision Meeting

**Date of meeting:** 13 September 2018

**Subject:** Fair Oak Road and Battenburg Avenue: double yellow line proposals (TRO 53B/2018)

**Report by:** Tristan Samuels, Director of Regeneration

**Wards affected:** Milton, Copnor

**Key decision:** No

**Full Council decision:** No

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## 1. Purpose of report

- 1.1. To consider public responses to proposed new double yellow lines in Fair Oak Road and a proposed reduction of double yellow lines in Battenburg Avenue.

Appendix A: The public proposal notice and plans for TRO 53/2018 (pages 5-6)  
Appendix B: Public views submitted (pages 7-12)

## 2. Recommendation

- 2.1. That the double yellow lines proposed under TRO 53/2018 in Fair Oak Road are implemented as advertised;
- 2.2. That the reduction of double yellow lines proposed under TRO 53/2018 in Battenburg Avenue, near its junction with Randolph Road, is not implemented in full, but that;
- 2.2.1 The 6-metre length of double yellow lines is removed from across the driveway between Nos.38 and 40 Battenburg Avenue

## 3. Background

- 3.1 Parking restrictions are considered and may be proposed where concerns are raised by residents, councillors, the public and/or emergency, public or delivery services in relation to road safety and traffic management.
- 3.2 Existing parking restrictions can be reviewed at the request of residents and/or councillors, and proposals may be put forward for public consultation as a result.

**3.3** A number of traffic regulation orders are put forward each year in direct response to such concerns and requests, and should objections be received, a decision by the Traffic & Transportation Cabinet Member is required to be made at a public meeting.

**3.4** Fair Oak Road: Double yellow lines have been proposed for the east side of Fair Oak Road between its junctions with Oakdene Road and Cheriton Road. This follows concerns raised by residents over vehicles parking on both sides of Fair Oak Road, partly on the footway and sometimes leaving insufficient carriageway width to travel through to Cheriton Road.

These images were provided by one of the residents concerned to demonstrate the issue:



**3.5** Battenburg Avenue: A resident suggested that the double yellow lines either side of Battenburg Avenue's junction with Randolph Road could be reduced. Proposals were put forward to remove some of the restrictions, to be consistent with similar junctions and in favour of on-street parking.

#### **4. Consultation and notification**

**4.1** Statutory 21-day consultation and notification under Traffic Regulation Order (TRO) 53/2018 took place 7-28<sup>th</sup> June 2018.

**4.2** Fair Oak Road: 2 x support, 1 x objection  
Battenburg Avenue: 0 x support, 3 x objections

**4.3** Traffic Regulation Orders can be made in part. Therefore, the remaining proposals under TRO 53/2018 which received no objections have been brought into operation under TRO 53A/2018. Approval of the proposal for Fair Oak Road would mean a separate order (TRO 53B/2018) facilitating the double yellow lines.

#### **5. Reasons for the recommendations**

**5.1** The information and concerns received from residents have informed the recommendations. Responses are reproduced at Appendix B on pages 7-12.

- 5.2** Fair Oak Road: The proposal responds to residents' concerns about vehicular access along Fair Oak Road and through to the houses in Cheriton Road when vehicles park on both sides of the road, and about visibility of traffic approaching from the bend including when exiting from the parking area to the rear of No.8 (coach house).

The proposed double yellow lines on the east side of Fair Oak Road between Oakdene Road and Cheriton Road therefore aim to maintain traffic flow, manage the parking and improve visibility of traffic approaching from the bend. The west side of Fair Oak Road can accommodate the most parking, and restricting the east side also reduces the potential for vehicles to obstruct access to properties when parking on the footway.

- 5.3** Battenburg Avenue: The recommendation takes into consideration the concerns of residents, and suggestions regarding part-implementation of the proposed reduction of double yellow lines.

## **6. Equality Impact Assessment**

- 6.1** An EIA is not required as the recommendations do not have a disproportionate negative impact on any of the specific protected characteristics as described in the Equality Act 2010. Parking restrictions apply to all motorists regardless of age, gender, disability, race, religion, sexual orientation etc., and they can promote improved access, road safety and traffic management for all.

## **7. Legal Implications**

- 7.1** It is the duty of a local authority to manage their road network with a view to achieving, so far as may be reasonably practicable having regard to their other obligations, policies and objectives, the following objectives:
- (a) securing the expeditious movement of traffic on the authority's road network; and
  - (b) facilitating the expeditious movement of traffic on road networks for which another authority is the traffic authority.
- 7.2** Local authorities have a duty to take account of the needs of all road users, take action to minimise, prevent or deal with congestion problems, and consider the implications of decisions for both their network and those of others.
- 7.3** A proposed TRO must be advertised and the statutory consultees notified and given a 3-week period (21 days) in which to register any support or objections. Members of the public also have a right to object during that period. If objections are received to the proposed order the matter must go before the appropriate executive member for a decision whether or not to make the order, taking into account any comments received from the public and/or the statutory consultees during the consultation period.

## **8. Director of Finance's comments**

- 8.1** The recommendations contained within this report do not have any adverse impact on the revenue budget and will be funded within the existing 2018/19 cash limits.

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Signed by:  
Tristan Samuels  
Director of Regeneration

**Background list of documents: Section 100D of the Local Government Act 1972**

The following documents disclose facts or matters, which have been relied upon to a material extent by the author in preparing this report:

<b>Title of document</b>	<b>Location</b>
6 emails / letters	Transport Planning team

The recommendation(s) set out above were approved/ approved as amended/ deferred/ rejected by ..... on .....

.....  
Signed by:  
Councillor Lynne Stagg, Cabinet Member for Traffic and Transportation



**THE PORTSMOUTH CITY COUNCIL (VARIOUS ROADS) (WAITING RESTRICTIONS, AND AMENDMENTS) (NO.53) ORDER 2018**

7 June 2018: Notice is hereby given that Portsmouth City Council proposes to make the above Order under sections 1 – 4, 32, 35, 36 and 53 of the Road Traffic Regulation Act 1984 ('the 1984 Act'), as amended, and parts III and IV of schedule 9 to the 1984 Act, to effect:

**A) NO WAITING AT ANY TIME (double yellow lines)**

- |                             |                                                                                                                                                                                                                                                                             |
|-----------------------------|-----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| <b>1. Blackfriars Close</b> | The southeastern end                                                                                                                                                                                                                                                        |
| <b>2. Central Street</b>    | North side, extend the double yellow lines over the cobbles to the dead end                                                                                                                                                                                                 |
| <b>3. Credenhill Road</b>   | East side, 10m north and 5m south of the junction with Rapson Close                                                                                                                                                                                                         |
| <b>4. Fair Oak Road</b>     | East side between the junctions of Oakdene Road and Cheriton Road; approximately 98m opposite the park                                                                                                                                                                      |
| <b>5. Farlington Avenue</b> | (a) Southwest side, a 12m length opposite Birkdale Avenue (between the dropped kerb and green cycle lane)<br>(b) Northeast side, a 20m length between Birkdale Avenue and the green cycle lane<br>(c) Northeast side, an 8m length between the cycle lane and Give Way line |
| <b>6. Hyde Park Road</b>    | South side, a 4m length between the parking bays outside Hyde Park House                                                                                                                                                                                                    |
| <b>7. Kenilworth Road</b>   | West side, a 3m length southwards from St Simon's Road junction                                                                                                                                                                                                             |
| <b>8. Moorings Way</b>      | (a) North side, a 58m length opposite even Nos.80-92 (from Shore Avenue cycle path eastwards to the bus stop)<br>(b) North side, a 23m length eastwards from the bus stop to the flat chicane                                                                               |
| <b>9. Petworth Road</b>     | A 3m length at the southern end to discourage double-parking                                                                                                                                                                                                                |
| <b>10. Rapson Close</b>     | Both sides, 5m eastwards from Credenhill Road junction                                                                                                                                                                                                                      |
| <b>11. St Simon's Road</b>  | South side, a 4m length westwards from Kenilworth Road junction                                                                                                                                                                                                             |

**B) CHANGE FROM LOADING BAY TO:**

**NO WAITING AND NO LOADING AT ANY TIME (double yellow lines, double yellow kerb stripes)**

- |                        |                                                                      |
|------------------------|----------------------------------------------------------------------|
| <b>1. Marmion Road</b> | South side, the 5m length east of Richmond Road (outside Victoriana) |
|------------------------|----------------------------------------------------------------------|

**C) CHANGE FROM 1-HOUR LIMITED WAITING TO:**

**LOADING ONLY 8AM-6PM**

- |                        |                                                                           |
|------------------------|---------------------------------------------------------------------------|
| <b>1. Marmion Road</b> | South side, a 9m length east of Richmond Rd outside Victoriana and No.80) |
|------------------------|---------------------------------------------------------------------------|

**D) REDUCTION OF NO WAITING AT ANY TIME (double yellow lines)**

- |                             |                                                                                                                                                              |
|-----------------------------|--------------------------------------------------------------------------------------------------------------------------------------------------------------|
| <b>1. Battenburg Avenue</b> | (a) North side, a 15m length west of Randolph Road, from halfway in front of No.1 up to No.7<br>(b) South side, an 11m length from outside No.38 up to No.40 |
| <b>2. Neville Road</b>      | West side, a 4m length north of Hayling Avenue opposite the pond/park                                                                                        |

**E) CHANGE FROM BUS STOP CLEARWAY TO:**

**NO WAITING AT ANY TIME (double yellow lines)**

- |                          |                                                                                                                |
|--------------------------|----------------------------------------------------------------------------------------------------------------|
| <b>1. Hayling Avenue</b> | North side, the majority of the bus stop east of Neville Road, leaving a 5m length unrestricted opposite No.84 |
|--------------------------|----------------------------------------------------------------------------------------------------------------|

**F) CHANGE FROM NO WAITING AT ANY TIME (double yellow lines) TO:**

**NO WAITING MON-FRI 8AM-6PM (single yellow line)**

- |                          |                                                                                         |
|--------------------------|-----------------------------------------------------------------------------------------|
| <b>1. Hayling Avenue</b> | (a) North side, a 5m length opposite No.94<br>(b) South side, a 5m length outside No.76 |
|--------------------------|-----------------------------------------------------------------------------------------|

**G) CHANGE FROM NO WAITING AT ANY TIME (double yellow lines) TO:**

**NO WAITING 8AM-6PM (single yellow line)**

- |                           |                                                                    |
|---------------------------|--------------------------------------------------------------------|
| <b>1. Priory Crescent</b> | North-west side, a 5m length between Vernon Ave and Carisbrooke Rd |
|---------------------------|--------------------------------------------------------------------|

**H) REMOVAL OF 1-HOUR LIMITED WAITING MON-SAT 8AM-6PM**

- |                     |                                                              |
|---------------------|--------------------------------------------------------------|
| <b>1. Liss Road</b> | North side, the 16m bay west of Winter Road alongside No.133 |
|---------------------|--------------------------------------------------------------|

PLANS: A4 (Fair Oak Road) and D1 (Battenburg Avenue)



Appendix B: Public views

## **1. FAIR OAK ROAD**

### **a) Resident, Cheriton Road**

I live in Cheriton Road, off Fair Oak Rd. We have recently been experiencing issues getting to our road via the ONLY way in and out which is Fair Oak Rd. It seems to have become increasingly worse the past few months.

Cars park opposite the residents houses along with many professional dog walker vans and commercial vehicles by St James Green. I have a family of 7 so I own a large 8 seater van and a few weeks ago I could barely get my vehicle through the gap left between cars !! I had to fold down my wing mirror and edge inch by inch to get through to my own road. In the event of an emergency there is no way an ambulance or fire crew could get through to us in Cheriton Road. I know I wasn't the only person who struggled to get through that day.

After speaking to other local neighbours this problem is occurring more and more. This is not an isolated incident. Other residents in Fair Oak Rd have actually politely approached the drivers of the vehicles who have parked inconsiderately..... only to be told they can park wherever they want!! There is no road markings yellow lines or entrance markings anywhere so it's a free for all.

I know for neighbours using the garages behind the coach house that it has been very difficult at times to see and manoeuvre through that entrance/exit. Even for people in a normal size car let alone larger vehicles.



This is only going to get worse with the pending construction works on St James Hospital and potential heavy vehicles passing through this tiny road! Which is absolutely an absolutely ridiculous idea to be frank!

I am extremely concerned for the safety of myself and my children walking in the area with cars parked all over the pavement on this estate. It is dangerous and obstructs the view of the road as well as stopping me and others simply being able to drive home into our own road with ease.

Please pay this your urgent attention.

### **b) Resident, Fair Oak Road**

As you are aware the St James Hospital development is steadily moving forward and at some point it would appear Fair Oak Road will be opened up and there will be construction traffic which will add to our current issue.

We already have a constant stream of Commercial Dog Walkers parking to access the green and now we have an HMO that has been granted which in turn has increased both commercial vehicles and cars in the estate.

There are currently no yellow lines, keep clear signs or permit parking and we as residents are consistently suffering from inconsiderate parking, e.g. Congestion around the green entrance parking across from the Coach House entrance making it difficult for vehicles to get in and out. Also the gaps being left between cars on either side of the road means emergency services would not get through which is a massive concern should they be needed.

Whilst we understand it is a public highway and therefore no parking restrictions, we have tried as neighbours to politely raise this issue with the offending vehicle owners but have just been met with ignorance sadly.

### **c) Resident, Fair Oak Road**

This action is in response to recent concerns expressed by residents of the estate of blockages at/or near the entrance to the shared courtyard between the properties 8 and 10 Fair Oak Road.

This courtyard provides one garage space and one off street parking bay for the residents of 4,6,8,10,12 and 14 Fair Oak Road. Access is required at all times of the day for residents and visitors to these properties.

I object to the proposal on grounds of access to this shared courtyard and potential safety issues for pedestrians in the estate. On behalf of all of these properties I recommend that the order is reconsidered.

Applying double yellow lines to the east side of the carriageway will force all of those parking to do so on the west side of the carriageway. This hinders access for those entering the estate on the west carriageway and turning right to enter the shared courtyard blocking access for a minimum of 6 vehicles.

It also seems to be bad practice to force vehicles entering the estate onto the wrong side of the carriageway passing the courtyard and the junction with Cheriton Road giving those leaving cheriton less time to react to approaching vehicles.

This is a particular concern for myself and my neighbour at 10 who own/regularly use camper vans with longer wheel bases than cars making them more difficult to manoeuvre - the proposed order makes turning a van into the courtyard impossible based on past experience when park users parked directly opposite the courtyard entrance. Approaching from the North and entering the courtyard by turning left is even more challenging when cars are parked on the west side.

My insurance for my van is based on parking on the driveway in the courtyard and given its value it would be a great inconvenience, cost and concern to have to park on the street.

If yellow lines were to be applied it would be preferable to do so in such a way to maintain the current parking arrangements without creating an obstruction for the courtyard (see attached sketch annotated with the following points:)

1. Apply yellows on east carriageway between No18 and No 10 down to a point just north of the courtyard.
2. Apply yellows on west carriageway between No10 and No2.
3. Apply marking denoting an entranceway to the courtyard preventing blockage/obstructions in conjunction with yellow lines on the west side. A minimum of a car length clearance either side of the courtyard would be required as a minimum.
4. Courtyard shown in red.



Your comments suggest that maximising available parking on the west of Fair Oak to alleviate some residents concern take precedence over my concern of being able to access the parking to the rear of my property via the coach house.

I think that with the appropriate demarcation applied the current parking arrangements can prevail with road marking being used to prevent park users blocking access for Cheriton and the shared access for 4,6,8,10,12 and 14. The proposal creates more problems than it fixes and should be reconsidered.

Have you heard from other residents?

#### Officer response

The proposal responds to residents' concerns, received independently, about vehicular access along Fair Oak Road through to the houses in Cheriton Road when vehicles park on both sides of the road, and about visibility of traffic approaching from the bend including when exiting from the parking area to the rear of No.8 (coach house) - this is the shared courtyard you refer to.

The proposed double yellow lines on the east side of Fair Oak Road between Oakdene Road and Cheriton Road therefore aim to maintain traffic flow, manage the parking and improve visibility of traffic approaching from the bend. The west side of Fair Oak Road can accommodate the most parking, and restricting the east side also reduces the potential for vehicles to obstruct access to properties when parking on the footway.



We are unable to add further double yellow lines to the proposal without re-consulting, and the current proposal responds to the concerns raised by residents. If issues arise from preventing vehicles from using both sides of the road for parking, then an appropriate proposal could be put forward in direct response to those issues.

The main concerns relate to insufficient carriageway being available when vehicles park on both sides, using part of the footway. There is a related concern that the fire service or an ambulance would not be able to access the properties in Fair Oak Road or Cheriton Road due to that practice.

Parking restrictions such as double yellow lines are considered for the purposes of road safety and managing traffic, and not specifically for improving access to private parking. This is so that the integrity of existing restrictions is maintained, and that resources and funding are focused where they are needed most for the benefit of all road users.

Currently vehicles can park opposite and either side of the entrance to the parking area via the coach house, which can make it difficult to see vehicles approaching from the right in

particular. Whilst the proposed double yellow lines would improve visibility when exiting the parking area, it is not the sole purpose of the proposal.

As you can imagine, there are a great many driveways, hard-standings and garages throughout the city that are difficult to access due to parking taking place either side and opposite, requiring additional vehicle manoeuvres to gain access. This is particularly noticeable in the narrow roads.

Residents' views will inform any decisions that are made, and residents will have a further opportunity to have their say at the public decision meeting.

## **2. BATTENBURG AVENUE**

### **a) Residents, Battenburg Avenue**

Dear Sir

We wish to object to the section D 1 (b) of this order relating to the removal of 11 metres of double yellow lines between number 38 and 40 Battenburg Avenue.

We feel the complete removal of these waiting restrictions will create several problems. If parking was allowed outside of number 38, vehicles travelling west down Battenburg Avenue would have serious siting issues of the junction with Randolph Road.

There could also be an issue with safe access to the double drive between number 38 and 40 if vehicles were allowed to park outside number 38 with the speeding traffic down this road.

Battenburg avenue is a busy through route between Copnor Road and London Road and its junction with Randolph Road has a history of accidents.

We are aware of three of these resulting in resident's cars being written off. There have also been many other accidents involving less serious damage to vehicles.

That is only the incidents we are aware of, I'm sure there are more that we are not aware of.

This junction also has a great deal of pedestrian and bicycle movement each day, to Mayfield school, Cliffdale Primary, Willows children's centre and the Battenburg clinic.

There are also many "near misses" on a daily basis.

We feel a more suitable option would be to remove 6 metres of double yellow lines outside of the drive between number 38 and 40 and to leave the remaining lines to the junction.

### **b) Resident, Battenburg Avenue**

I am writing to to inform you of my objection to the above proposal, for the reasons as below that I urge the council to consider.

When travelling south in Randolph Road and turning right (West) into Battenburg Avenue - there is a curve in the road between numbers 1 and 3 (Battenburg Avenue). This curve restricts a car drivers view of traffic that is travelling East along Battenburg avenue.

If the parking restrictions were lifted in this area, it would restrict a drivers view exiting Randolph road even further, making it potentially very dangerous.

I appreciate that the council are under pressure to provide parking were suitable, however, with this in mind I would like to inform you that I have been a resident of Randolph road for 40 years and can remember when there were previously no parking restrictions on this section of road (Between 1 and 7 Battenburg avenue). I can also recall several crashes that took place in this exact area.

**c) Residents, Battenburg Avenue (same letter as above, different address/signatures)**

Dear Sir

We wish to object to the section D 1 (b) of this order relating to the removal of 11 metres of double yellow lines between number 38 and 40 Battenburg Avenue.

We feel the complete removal of these waiting restrictions will create several problems. If parking was allowed outside of number 38, vehicles travelling west down Battenburg Avenue would have serious siting issues of the junction with Randolph Road.

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That is only the incidents we are aware of, I'm sure there are more that we are not aware of.

This junction also has a great deal of pedestrian and bicycle movement each day, to Mayfield school, Cliffdale Primary, Willows children's centre and the Battenburg clinic. There are also many "near misses" on a daily basis.

We feel a more suitable option would be to remove 6 metres of double yellow lines outside of the drive between number 38 and 40 and to leave the remaining lines to the junction.

*(End of report)*



**Title of meeting:** Cabinet Member for Traffic and Transportation

**Date of meeting:** 13 September 2018

**Subject:** Proposed Residents' Parking Zone: FH Twyford Avenue (TRO 76/2018)

**Report by:** Tristan Samuels, Director of Regeneration

**Wards affected:** Nelson, Hilsea

**Key decision:** No

**Full Council decision:** No

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**1. Purpose of report**

- 1.1** The purpose of this report is to consider the public response to the proposed residents' parking zone at the northern end of Twyford Avenue.

Appendix A: Resident's Parking survey results (pages 4-5)

Appendix B: Public proposal notice (pages 6-7)

Appendix C: Public responses (pages 8-11)

**2. Recommendations**

It is recommended that:

- 2.1** **the residents' parking zone proposed under Traffic Regulation Order 76/2018 is implemented as advertised (FH Permit Holders Only 2pm-10pm)**

**3. Background**

- 3.1** The northern end of Twyford Avenue, adjacent to the Mountbatten leisure centre, appeared on the Residents' Parking Programme that was approved in 2015.
- 3.2** The location was surveyed (stage 1 informal survey) in relation to Residents' Parking in June-July 2018. Eight of the 18 survey forms were completed and returned, showing 75% of those who responded in favour of permit parking and 25% against.
- 3.3** Therefore a Residents' Parking Zone was formally proposed via Traffic Regulation Order (stage 2 formal consultation) 76/2018 in July 2018, taking into consideration the information provided by residents in response to the informal survey.

**4. Consultation**

**4.1** There is a 3-stage process in relation to Residents' Parking Zones:

Stage 1: Informal survey	Gathers information from residents about parking problems, and determines whether or not to proceed to Stage 2
Stage 2: Formal statutory consultation via a traffic regulation order (TRO)	Public consultation takes place on a proposed parking zone. Recommendations are made based on how residents respond
Stage 3: Decision by Cabinet Member	The response to the proposed parking zone is considered at a public decision meeting

**4.2** Following the positive response to the informal survey (stage 1 above) a formal statutory consultation was undertaken via TRO 76/2018 (stage 2 above) from 31 July - 22 August 2018 regarding a proposed parking zone.

**5. Reasons for recommendation**

**5.1** The information received from residents during the informal and formal consultations has been considered, and has informed the recommendation.

**5.2** The June/July 2018 survey showed:

- 75% of residents who responded were in favour of a permit scheme
- the majority of parking problems occur during the afternoon, evening and overnight
- the main reason given for the parking problems was close proximity to the Mountbatten Centre.

**5.3** The formal consultation under TRO 76/2018 proposed a Residents' Parking Zone to operate as FH Permit Holders Only between 2pm - 10pm daily, informed by the survey responses above.

**5.4** 4 responses were received to the formal TRO consultation: in support, objection and/or asking for clarification on some points raised (see pages 8-11 of this report)

**6. Equality impact assessment**

**6.1** An EIA is not required as the recommendations do not have a disproportionate negative impact on any of the specific protected characteristics as described in the Equality Act 2010. Disabled badge holders are exempt from permit restrictions within Residents' Parking Zones, provided the Blue Badge is clearly on display.

## **7. Legal implications**

- 7.1** It is the duty of a local authority to manage their road network with a view to achieving, so far as may be reasonably practicable having regard to their other obligations, policies and objectives, the following objectives:
- (a) securing the expeditious movement of traffic on the authority's road network; and  
(b) facilitating the expeditious movement of traffic on road networks for which another authority is the traffic authority."
- 7.2** Local authorities have a duty to take account of the needs of all road users, take action to minimise, prevent or deal with congestion problems, and consider the implications of decisions for both their network and those of others.
- 7.3** Traffic regulation orders (TROs) can be made for a number of reasons, including avoiding danger to persons or other traffic using the road or for preventing the likelihood of such danger arising, for preventing damage to the road or any building on or near the road, for facilitating the passage on the road of traffic (including pedestrians) or preserving or improving the amenities of the area through which the road runs.
- 7.4** A TRO may make provisions for identifying any part of the road to which any provision of the TRO is to apply by means of a traffic sign.
- 7.5** A proposed TRO must be advertised and the public given a 3 week consultation period where members of the public can register their support or objections. If objections are received to the proposed order the matter must go before the appropriate executive member for a decision whether or not to make the order, taking into account the comments received from the public during the consultation period.
- 7.6** Where a TRO is made the local authority must within 14 days publish a notice that the order has been made in a local newspaper. The notice must include amongst other things, where and when the order is available for inspection and that within six weeks following the making of the order that an application can be made to the High Court to question the validity of the order or any its provisions.
- 7.7** The local authority must take appropriate steps to ensure that adequate publicity about the order is given and must notify any person who has objected to the order (where such objection has not been withdrawn) that the order has been made.

## **8. Director of Finance's comments**

- 8.1** The costs associated with the advertising of the TRO and the set up costs (which includes signage and line marking) is estimated to be £1,600. This cost will be met from the On Street Parking budget.

- 8.2** The cost of administering, maintaining and enforcing the residents parking zone will be absorbed within the current On Street Parking budget and income generated from the zone will be remitted to the On Street Parking reserve.

.....  
Signed by:  
Tristan Samuels  
Director of Regeneration

**Background list of documents: Section 100D of the Local Government Act 1972**

The following documents disclose facts or matters, which have been relied upon to a material extent by the author in preparing this report:

<b>Title of document</b>	<b>Location</b>
Traffic and Transportation report July 2018	Portsmouth City Council website
Survey results July 2018	Reproduced within this report
4 emails	Reproduced within this report

The recommendation(s) set out above were approved/ approved as amended/ deferred/  
rejected by ..... on .....

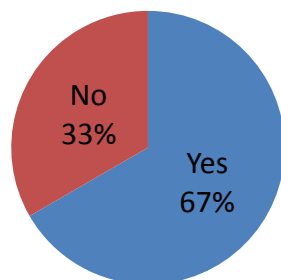
.....  
Signed by:  
Councillor Lynne Stagg  
Cabinet Member for Traffic and Transportation

Appendix A: Residents' parking survey results (July 2018)

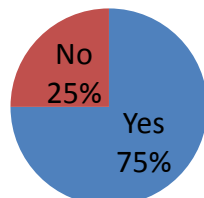
**TWYFORD AVENUE (northern end, odd numbered properties 313-347)**

Surveys distributed: 18  
Surveys completed: 8 (44%)

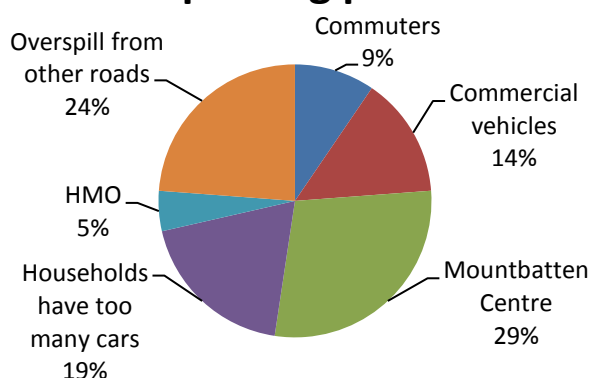
**Do you have parking problems in the road/area where you live?**



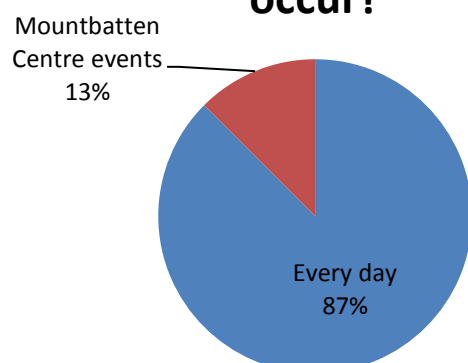
**Do you think a residents' parking scheme would be helpful to you and/or your visitors?**



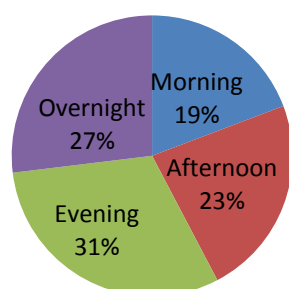
**What do you think is the reason for the parking problems?**



### When do the parking problems occur?



### What times do the parking problems occur?



Appendix B: Public proposal notice (TRO 76/2018)

**THE PORTSMOUTH CITY COUNCIL (TWYFORD AVENUE) (RESIDENTS' PARKING ZONE AND AMENDMENTS TO WAITING RESTRICTIONS) (NO.76) ORDER 2018**

31 July 2018: Notice is hereby given that Portsmouth City Council proposes to make the above Order under sections 1-4, 45, 51, 52 and 53 of the Road Traffic Regulation Act 1984. The effect would be as detailed below.

**SEND YOUR COMMENTS ON THESE PROPOSALS TO:**

[engineers@portsmouthcc.gov.uk](mailto:engineers@portsmouthcc.gov.uk)

**CURRENT PARKING CHARGES**

**Resident permits** -. A maximum of 2 Resident permits will be authorised per household each year unless capacity allows. Resident permits are electronic: paper permits are no longer issued

£30.00/year for first permit

£80.00/year for second permit

£550.00/year for third permit (if capacity allows)

**Visitor permits (for visitors to residents)**

£1.05 for 12 hours

£2.00 for 24 hours

**Business permits**

£130.00/year for first permit

£260/year for a second permit

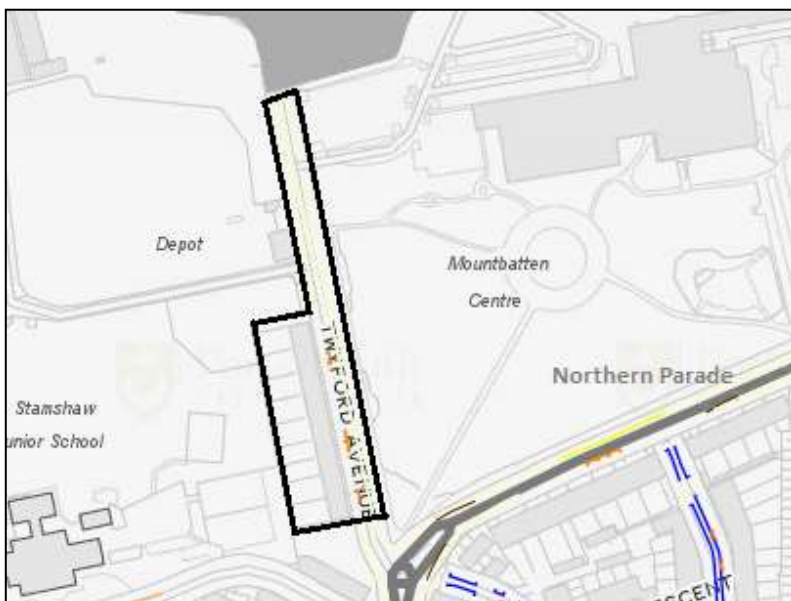
£550/year for each subsequent permit

**Replacement/amendment of permit** - £10.00 administration charge

**Blue Badge holders** and **motorcycles** are exempt from the parking zone restrictions.

Permits for goods vehicles are restricted to those with a gross vehicle weight of less than 3501kg and registered to an address within the parking zone. Permits are only issued to businesses operating within the parking zone.

**A) FH PARKING ZONE BOUNDARY**



**B) FH PERMIT HOLDERS ONLY 2PM-10PM**

**1. Twyford Avenue, northern end**

West side, the 109m length of unrestricted on-street parking northwards from outside No.313

**C) PERMIT ENTITLEMENT**

Nos. 313-347 (odd numbers) are eligible to apply for FH zone permits

**D) CHANGE FROM PROHIBITION OF WAITING AT ANY TIME (double yellow lines) TO:  
FH PERMIT HOLDERS ONLY 2PM-10PM**

**1. Twyford Avenue**

The west side, a 5m length northwards from No.347

**REASONS FOR THE ORDER**

The northern dead end of Twyford Avenue was included on the approved 2015 Residents' Parking Programme for survey. The June/July 2018 survey showed 75% of residents who responded to be in favour of a permit scheme, and that the majority of parking problems occur during the afternoon, evening and overnight. The main reason given for the parking problems was close proximity to the Mountbatten Centre.

To view this public notice on Portsmouth City Council's website, search 'traffic regulation orders 2018' and select 'TRO 76/2018'. A copy of the draft order including the statement of reasons is available for inspection at the main reception, Civic Offices during normal open hours.

Persons wishing either to object to or support these proposals may do so by sending their representations via email to **engineers@portsmouthcc.gov.uk** or post to Nikki Musson, Transport Planning, Portsmouth City Council, Civic Offices, Portsmouth PO1 2NE, quoting ref **TRO 76/2018** by **22 August 2018** stating the grounds of objection/support.

Under the provisions of the Local Government (Access to Information) Act 1985, any written representations which are received may be open to inspection by members of the public. These are anonymised. If the proposals require approval at a public decision meeting, representations are included in the associated published report, and are also anonymised.

Pam Turton, Assistant Director of Regeneration (Transport)  
Portsmouth City Council, Civic Offices, Guildhall Square, Portsmouth PO1 2NE

Appendix C: Public responses to the proposed parking zone (TRO 76/2018)

**1. Resident, Twyford Avenue**

I support the proposal for permit holders only

**2. Resident, Twyford Avenue**

In general I feel positive about the resident parking permit area. Here I have some suggestions and questions:

- 1) Is it possible to remove couple of meters of double yellow line from no.313 toward roundabout? (Yellow area on attached picture) Because of disabled parking space it is possible to fit 1.5 car and need couple or meters for second one and seems there are enough space.
- 2) Is it the way to convert to parking area not only west side but east as well from 313 till 347? We got there 2 sidewalks running in parallel anyway. Maybe it is good idea to remove one sidewalk and build parking area and improve second one.
- 3) I saw on old plans there was a road behind houses 313-347. Do you have any plans to build it in future? In this case all cars can be parked there but on main road it will be space for cycle path to new Tipner lake path.

Also can you explain more about 5m length double line from 347 (Point D on your proposal). Is it mean residents can park there? Why time is 2pm-10pm?

Officer comments

- 1) This has been considered, but would leave no visibility for vehicles exiting the car park onto Twyford Avenue by the roundabout, and would cause 2 vehicles to try and park, either overhanging the disabled bay or the double yellow lines. The gap between the double yellow lines and disabled bay is 6.5 metres:



- 2) Widening the road by reducing the public footway on the east side would be considered at a future date in conjunction with the redevelopment of Tipner East. There are currently no proposals to adjust the infrastructure of Twyford Avenue.
- 3) This area falls within the Stamshaw Junior School grounds, and there are currently no plans to build an access road there.



TRO 76/2018 includes a proposal to reduce the double yellow lines by 5m, bringing the space into the permit parking zone, proposed to operate 2pm-10pm due to the information provided by residents on when parking problems occur.

**3. Resident, Twyford Avenue**

With reference to (TRO 76/2018)

A couple of points I am concerned about...

1. There still will not be enough parking spaces for the number of cars as it is a very small stretch of road without double yellow lines.
2. Why is it only allowing parking at the end of the road after No 347, 2pm until 10pm? I work shift and do not get home till 12pm so I would still not have a space. At 10pm if you are parked there are you then supposed to hunt for a space?
3. Could the double yellow lines outside no.313 not be moved along slightly as between them and the disabled space there is only enough space for one and a half cars. Could this not be changed so there is space for two cars.

Officer comments

1. The 115m length will provide parking space for approximately 21 vehicles. There are 18 properties. Similar parking zones, such as Coniston Avenue in Copnor, Old Commercial Road in Landport and Portsmouth Road in Cosham, see positive results for residents as parking by non-residents is restricted.
2. The parking zone is proposed to operate the restriction of 'FH Permit Holders Only 2pm-10pm' and therefore the new space by No.347 would operate the same restriction should the double yellow lines be reduced as proposed. If you are parked in the space at 10pm (with an FH permit) there would be no need to find another space.
3. See response at No.1) within the previous box.

**4. Resident, Twyford Avenue**

I am not an avid fan of these type of schemes for the following reasons:

1. Despite paying for one or more residents parking permits there is no guarantee that any parking space will be available.
2. Without a paper indication that a vehicle is included in the scheme how can the parking zone be enforced without frequent monitoring by enforcement staff. I see these officers very rarely in this area and often only when major events are taking place at the Mountbatten centre. {Perhaps there is more chance of issuing tickets when visitors to this venue are struggling to find parking} This proposed scheme will not deter visitors to the Mountbatten centre taking a chance and parking in the zone anyway.
3. Is it intended to increase visits by enforcement officers to ensure compliance from vehicles not included in the scheme.
4. Why are Blue Badge holders exempt from the restriction. Would it not be fairer, where there are multiple Blue Badge bays in a street, that these households pay the standard

charge {1st payment rate} for the Blue Badge bay then the same {first permit rate} for any second vehicle. Some users of the Blue Badge scheme park their primary vehicle {ie: the vehicle registered in the Blue Badge scheme} outside of the Blue Badge bay in order that their second or third vehicle can then have a parking space available when these vehicles return to the street.

5. There are two vehicles within my household. I commute 50 miles out of the city of Portsmouth 5 days out of 7, often returning between 5 - 6pm {during the scheme operating hours} or sometimes between 2359 - and 0100 {outside of the scheme operating hours}. Were I to purchase first and second vehicle permits for £110.00 per year would I realistically be able to ever park in my road.

I voted NO to this scheme and do not see any advantage to my own household if it is implemented, other than generating additional revenue for the council. What would these extra funds be used for - perhaps it could pay for an increased presence of enforcement officers to manage any vehicles contravening the scheme.

#### Officer comments

1. The comment is noted. This information is included in the Council's survey form, to make residents aware that parking zones do not guarantee a parking space. However, as outlined in point 2) within the previous box, existing parking zones show that residents have more chance of parking outside or near their homes when a parking zone is in operation, than before.

2. Enforcement officers have access to vehicle and permit data via their electronic handheld devices. There is no free parking period proposed for the FH Twyford Avenue zone, and therefore enforcement staff would be able to issue a PCN to unauthorised vehicles within minutes of observing them. Details of the number of PCNs issued to vehicles within locations are available on request, and those who do request them are often surprised at the number of visits made by enforcement staff.

3. It is always possible to increase the number of enforcement visits where low levels of compliance are identified or reported.

4. It is Portsmouth City Council policy that Blue Badge holders are exempt from permit restrictions within Residents' Parking Zones. Blue Badge holders have a national exemption from Pay & Display and limited waiting, and may park on double and single yellow lines for up to 3 hours. Many Residents' Parking Zones include such restrictions and therefore a consistent approach has been adopted to avoid confusion and enable those who need to park close to their destination to do so.

5. See point 1 above. Whilst it is not possible to create a parking zone that will satisfy everyone in terms of their individual circumstances, residents report that it is easier to park with permit restrictions in place as parking by non-residents is reduced overall.

The resident's objection is noted. Yes, enforcement is one of areas funded by parking permits, along with permits, line-marking and signage, and ongoing maintenance of the parking zones and administration.

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*(End of Report)*